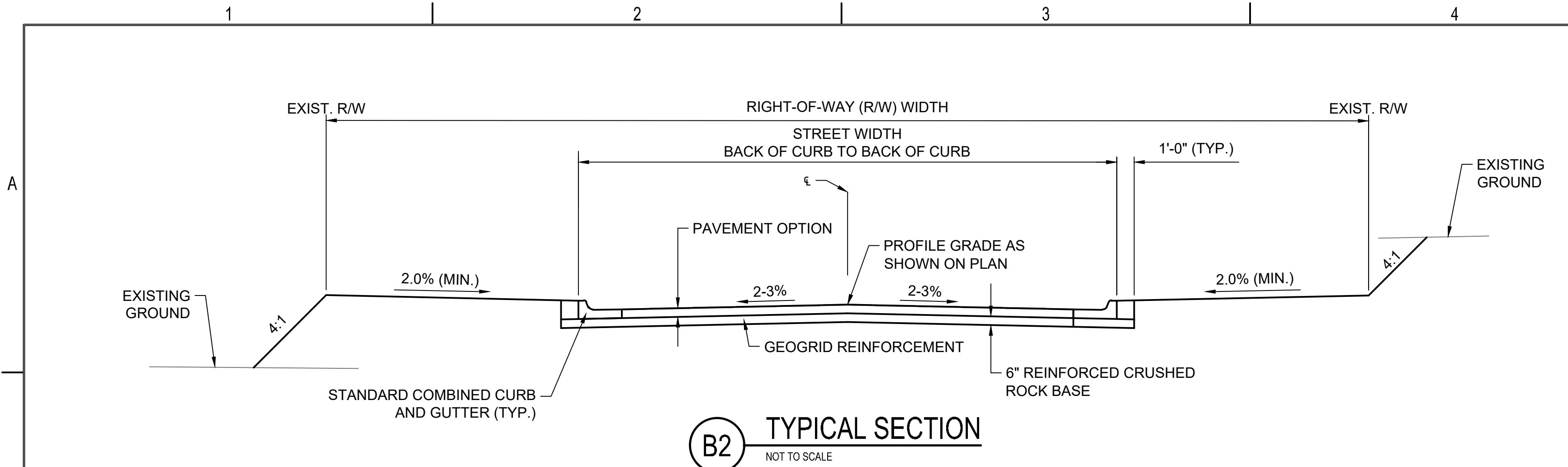


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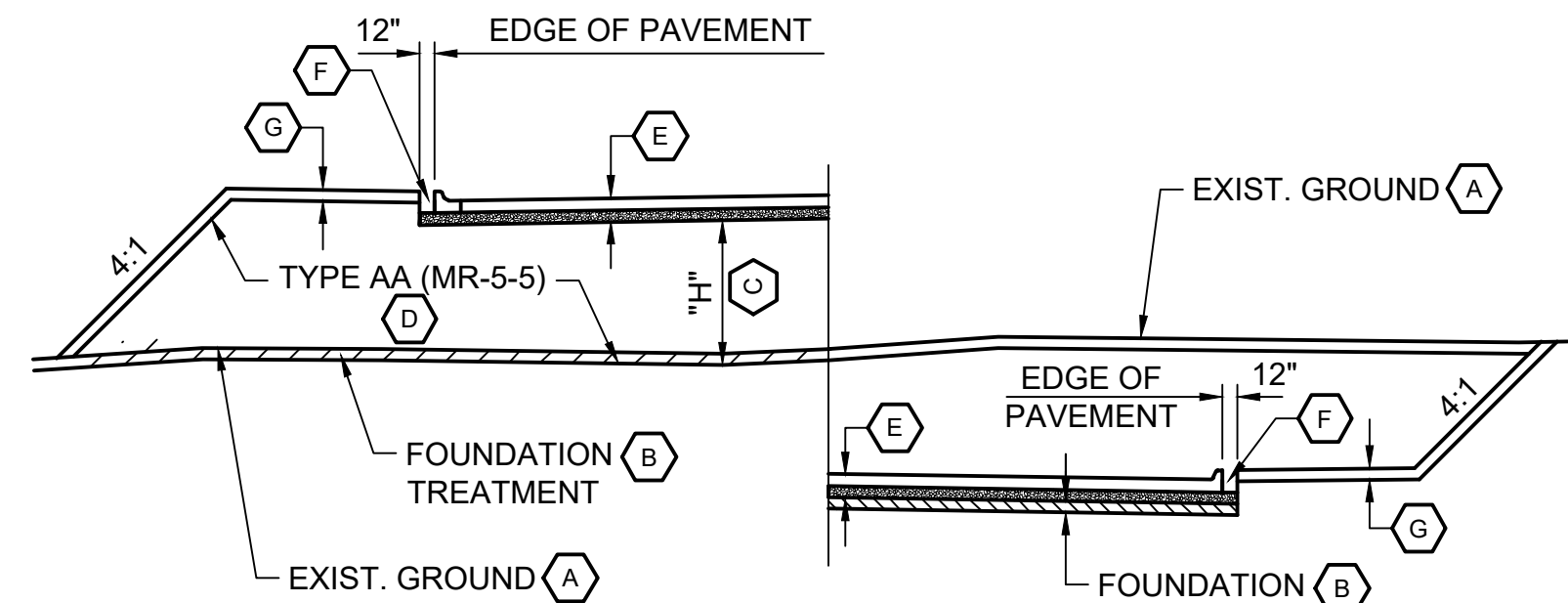
STREET WIDTH TABLE *	
STREET CLASSIFICATION	STREET WIDTH BK OF CB TO BK OF CB
ARTERIAL	**
COLLECTOR	41'
RESIDENTIAL	31'-35'

* PER CITY SUBDIVISION REGULATIONS, SECTION 16.06.08.

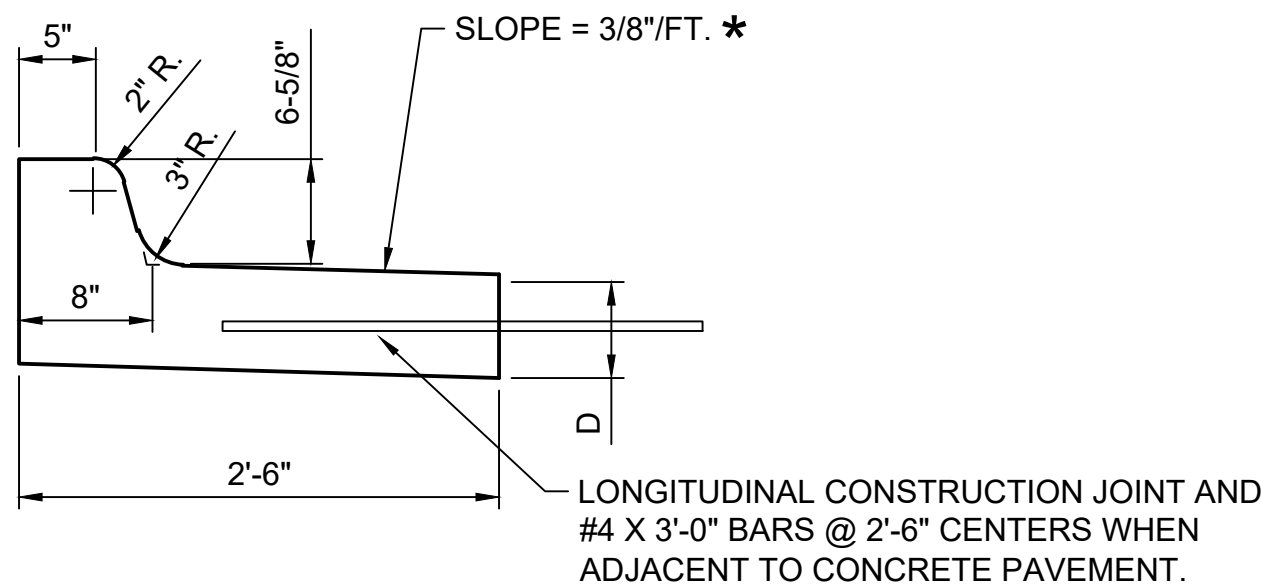
** SEE CITY SUBDIVISION REGULATIONS, SECTION 16.06.08.G AND H.

PAVEMENT THICKNESS TABLE			
PAVEMENT TYPE	RESIDENTIAL	SECONDARY	ARTERIAL
ASPHALTIC CONCRETE	5" MIN.	7" MIN.	8" MIN.
CONCRETE	6" MIN.	6" MIN.	7" MIN.

D= THICKNESS OF ADJACENT PAVEMENT



- A STRIP TOPSOIL, SEE RD599A.
- B SCARIFY AND COMPACT (6 INCHES) TO 95% ASTM D698.
- C "H" EQUALS FULL EMBANKMENT HEIGHT.
- D COMPACT TO 95% ASTM D698.
- E PAVEMENT SECTION, SEE TYPICAL SECTIONS THIS SHEET.
- F BACKFILL WITH SATISFACTORY SOIL. COMPACT TO 95% ASTM D698.
- G PLACE TOPSOIL, SEE RD599A (SALVAGE TOPSOIL).

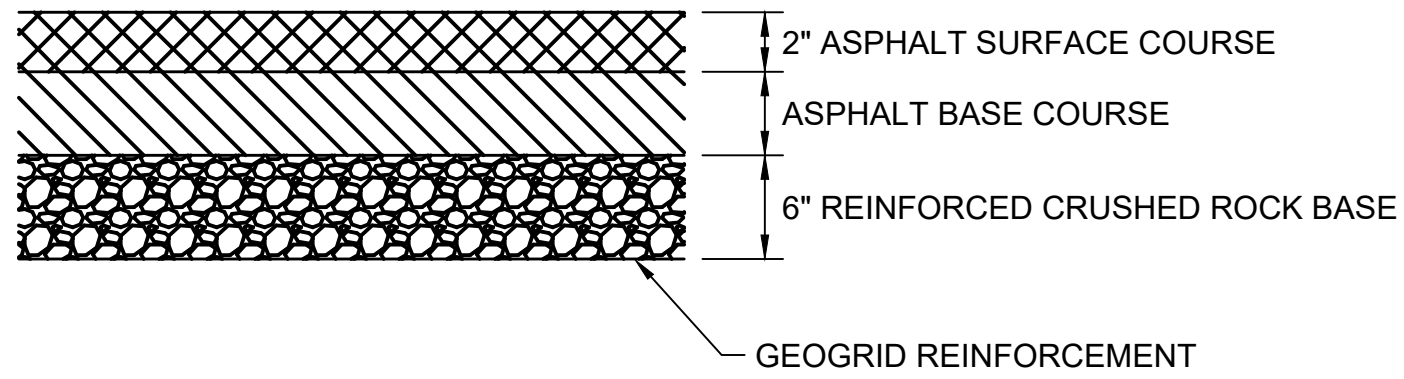
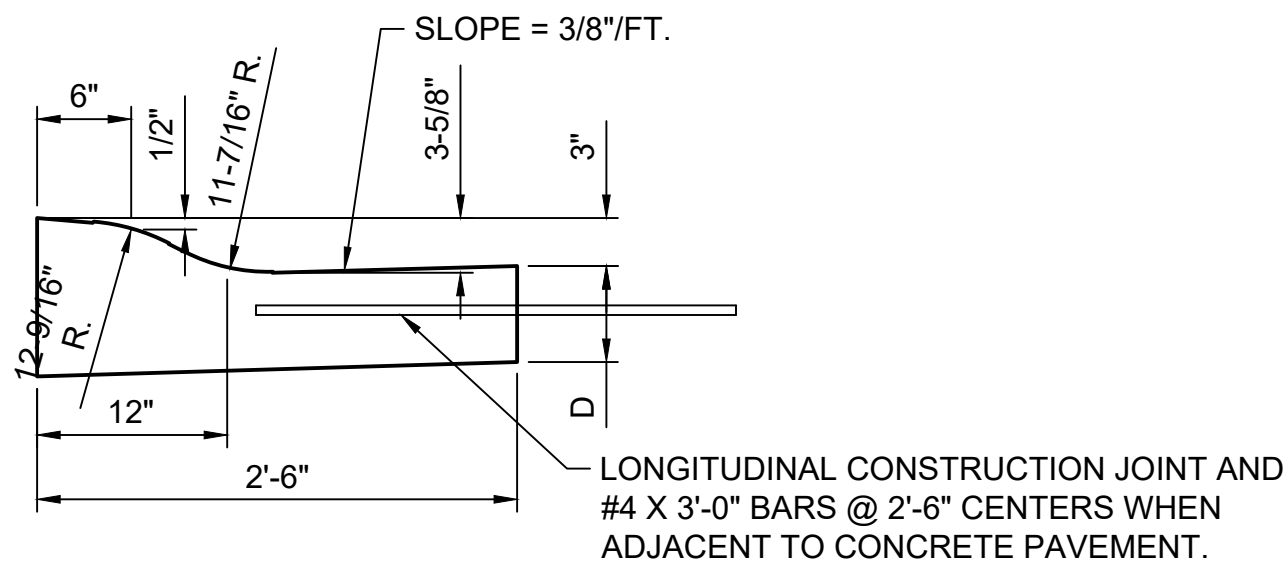


* SLOPE SAME AS ADJACENT PAVEMENT. CONTRACTOR SHALL
VERIFY DIRECTION OF SLOPE WITH FINAL GRADING.

D3

MEDIAN COMBINED CURB & GUTTER (6 5/8")

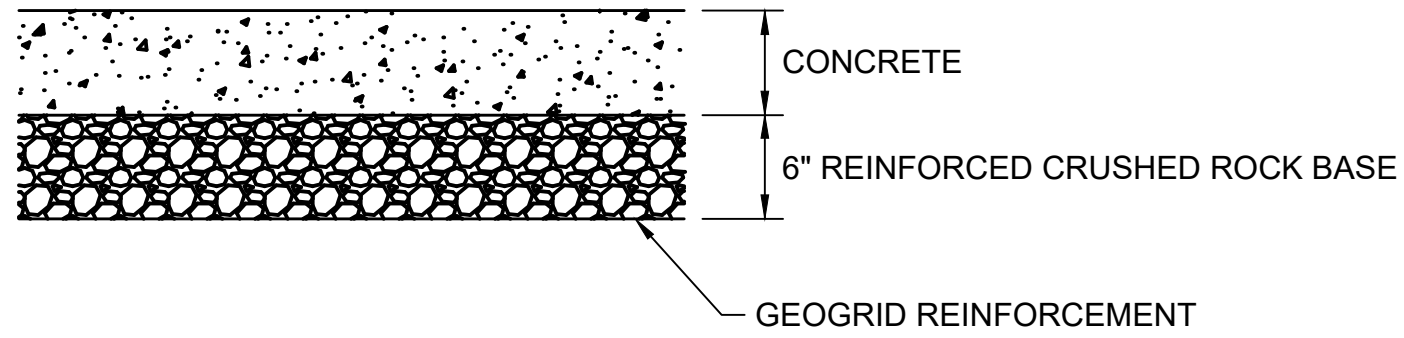
NOT TO SCALE



A5

PAVEMENT OPTION, ASPHALTIC CONCRETE

NOT TO SCALE

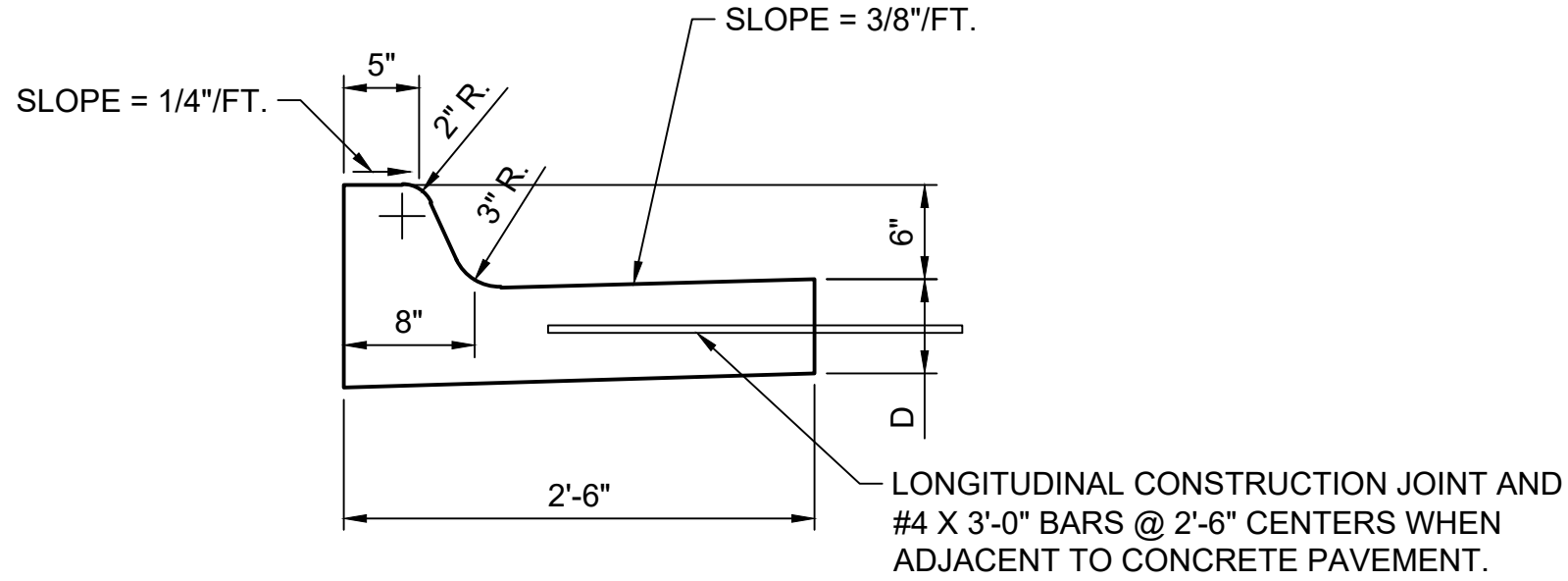


B5

PAVEMENT OPTION, CONCRETE

NOT TO SCALE

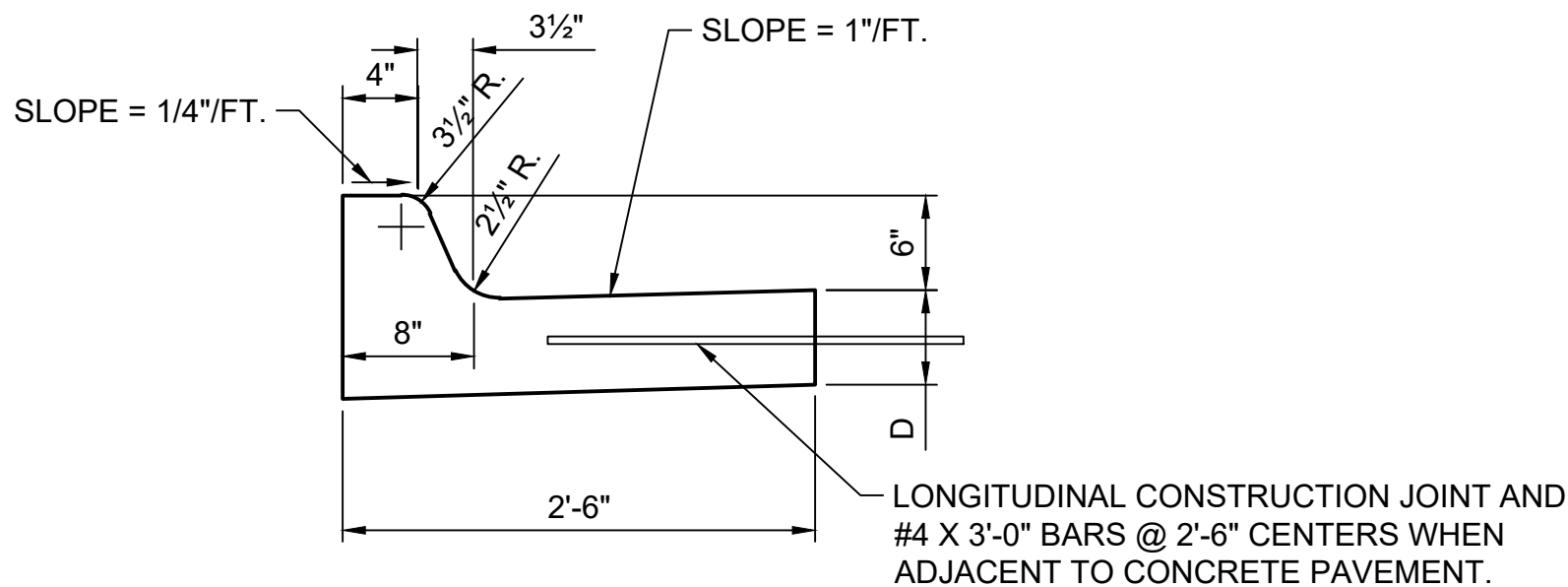
NOTES:
THICKNESS WILL VARY DEPENDING ON THE INTENDED USE. SEE TABLE BELOW FOR MINIMUM
PAVEMENT THICKNESS.



D5

STANDARD COMBINED CURB & GUTTER (6-5/8")

NOT TO SCALE



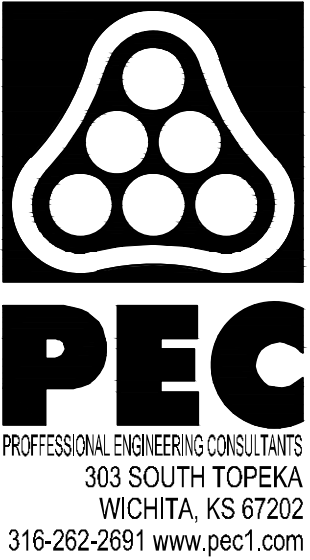
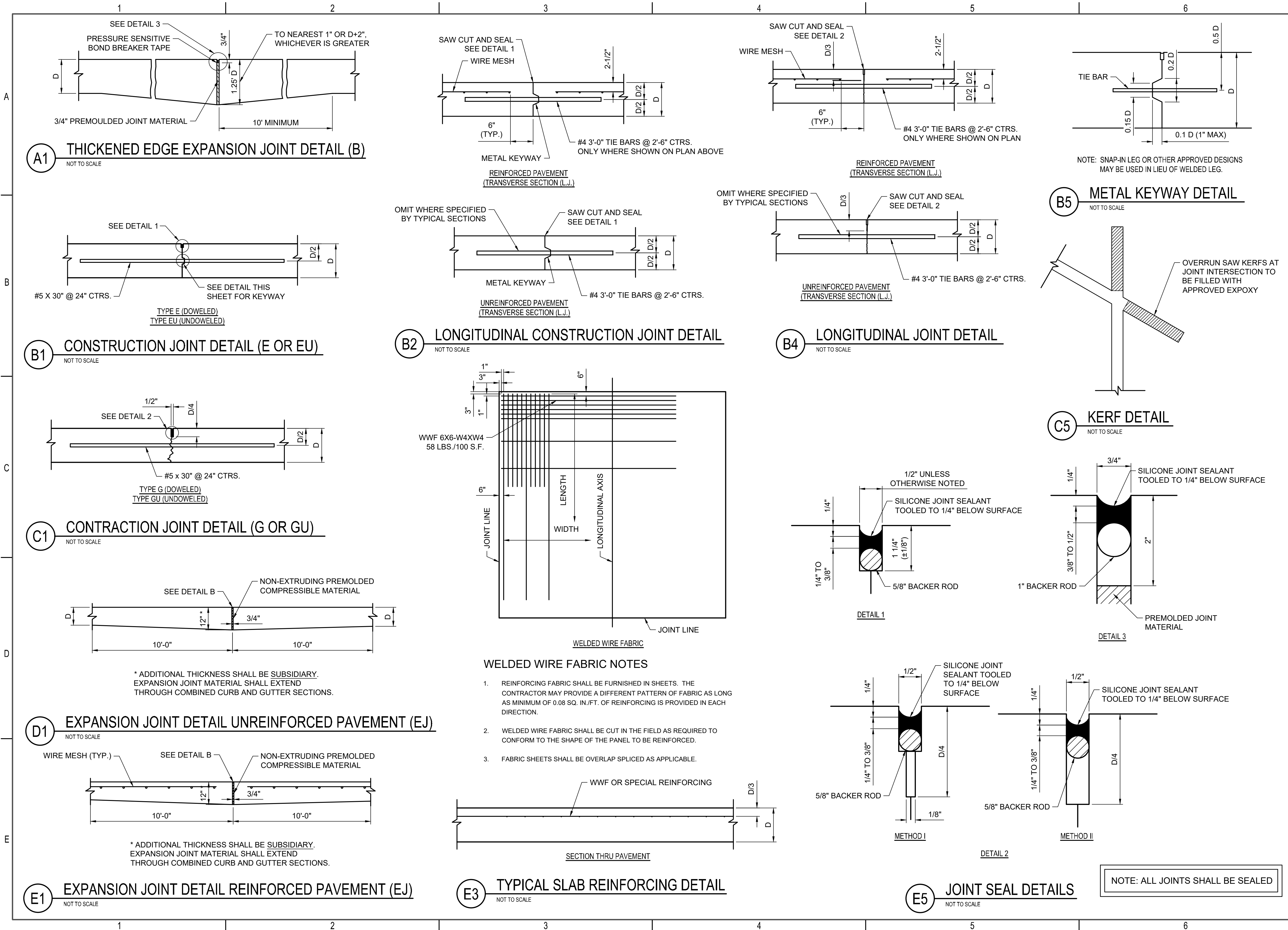
VALLEY CENTER
STANDARD DETAILS
VALLEY CENTER, KS

Issue:		
1	ISSUED FOR CONSTRUCTION	03/10/21
JOB NO.	217013-001	
DATE	10 MARCH 2021	
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TYPICAL PAVING SECTIONS

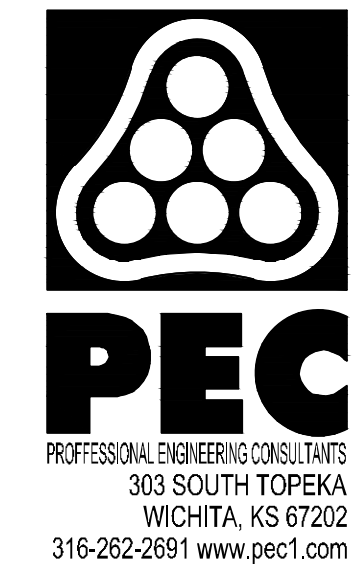
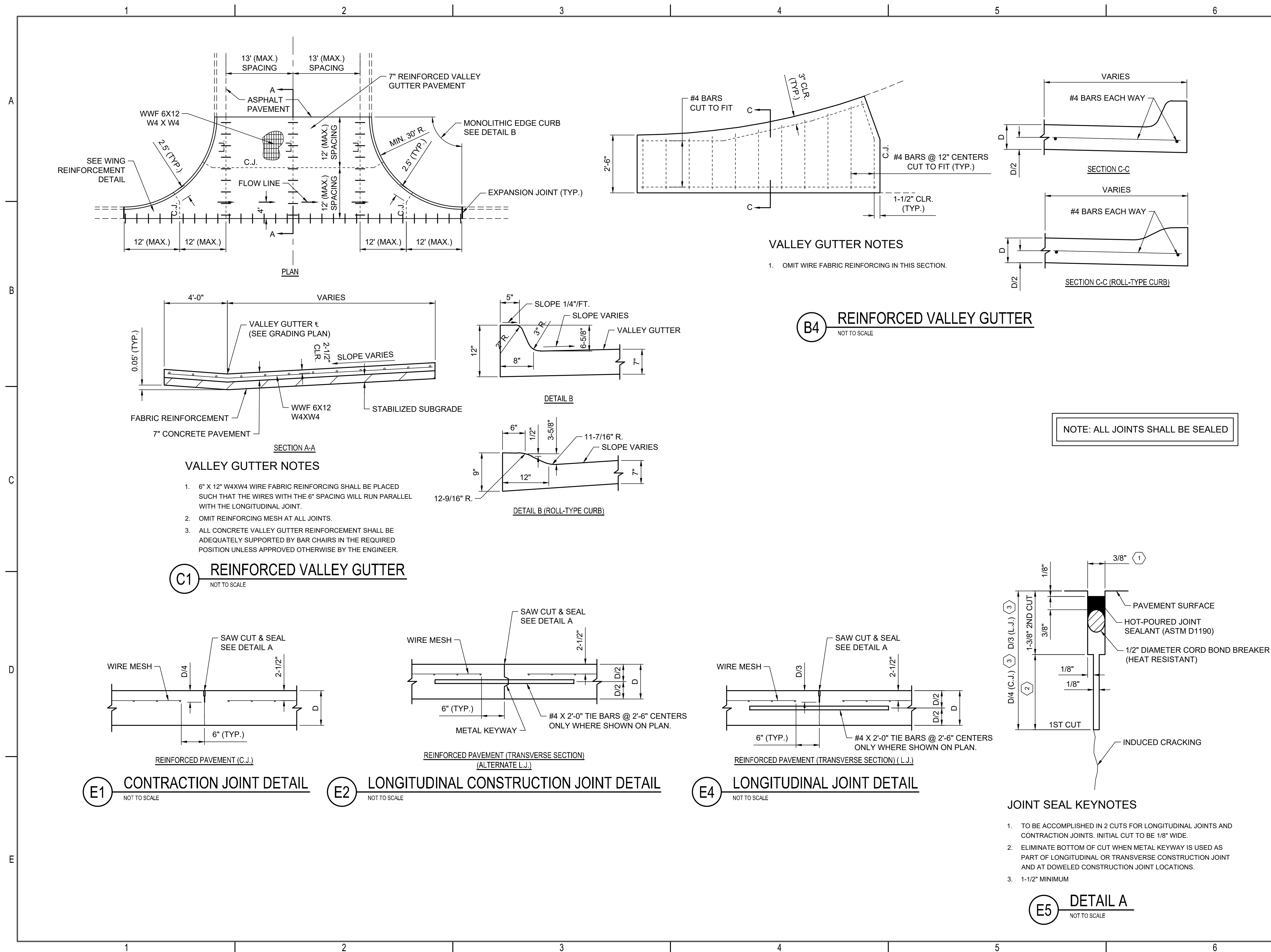
C-501

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 AND JOINTING DETAILS.DWG



VALLEY CENTER
 STANDARD DETAILS
 VALLEY CENTER, KS

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CONCRETE PAVEMENT AND JOINTING DETAILS		
C-502		



VALLEY CENTER
STANDARD DETAILS

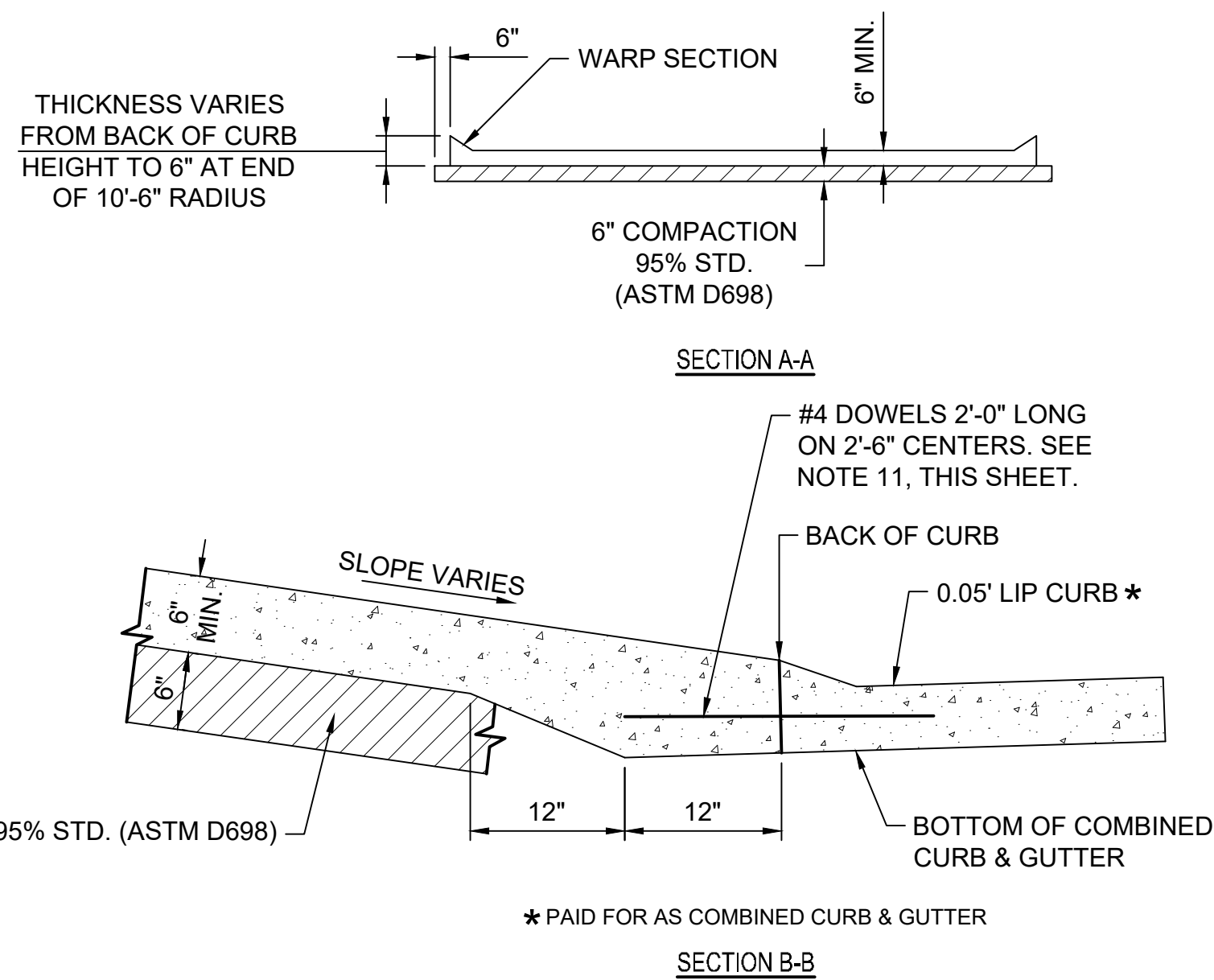
VALLEY CENTER, KS

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JOB NO.	217013-001
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DRAWN BY	DSB
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CONCRETE VALLEY GUTTER DETAILS

C-503



D3 CONCRETE DRIVE DETAILS



E3 JOINT DETAILS FOR CONCRETE DRIVES
NOT TO SCALE

REINFORCED CONCRETE DRIVE NOTES

1. CONCRETE DRIVE ENTRANCE SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF VALLEY CENTER STANDARD SPECIFICATIONS, SECTION 32 13 13 "CONCRETE PAVING."
2. THE CONTRACTOR SHALL PROVIDE A FULL DEPTH SAW CUT AT THE LIMITS OF CONSTRUCTION. SAW CUTS AT THE FLOWLINE OF EXISTING CURB AND GUTTER WILL NOT BE ALLOWED.
3. ONE LONGITUDINAL JOINT SHALL BE CONSTRUCTED ALONG THE CENTERLINE OF DRIVES HAVING A "W" DIMENSION OF 20' OR LESS. TWO LONGITUDINAL JOINTS SHALL BE CONSTRUCTED WITH EQUAL SPACINGS OF 10' OR LESS FOR DRIVES WITH A "W" DIMENSION GREATER THAN 20'.
4. JOINT SPACING SHALL BE ADJUSTED TO ALIGN WITH OUTER EDGE OF SIDEWALK WHERE SIDEWALK ABUTS DRIVE CONSTRUCTION.
5. ADDITIONAL THICKNESS OF DRIVE PAVEMENT AS INDICATED IN SECTION B-B SHALL NOT BE PAID FOR DIRECTLY, BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR DRIVEWAY PAVEMENT.
6. EXPANSION JOINTS ONE-HALF-INCH WIDE SHALL BE INSTALLED WHEN CONCRETE DRIVE ABUTS AN EXISTING CONCRETE DRIVE.
7. FOR BIDDING PURPOSES, ALL RESIDENTIAL DRIVEWAYS SHALL BE 6" IN THICKNESS AND SHALL BE REINFORCED. ALL COMMERCIAL/INDUSTRIAL DRIVEWAYS SHALL BE 8" IN THICKNESS AND SHALL BE REINFORCED. REINFORCEMENT SHALL BE 6" x 6" W2.9-W2.9 WELDED WIRE FABRIC. ALL REINFORCEMENT SHALL BE ADEQUATELY SUPPORTED BY BAR CHAIRS IN THE REQUIRED POSITION UNLESS APPROVED OTHERWISE BY THE ENGINEER.
8. DRIVE APPROACHES SHALL BE CONSTRUCTED TO THE STREET RIGHT-OF-WAY LINE OR AT THE LIMITS SHOWN ON THE PLANS AT A GRADE WHICH WILL ALLOW SMOOTH HOOKUP TO THE EXISTING DRIVEWAY. AT THE WRITTEN REQUEST OF THE PROPERTY OWNER, FURTHER GRADING, PAVEMENT REMOVAL, PAVING, ETC. ON PRIVATE PROPERTY MAY BE PERFORMED TO CONNECT TO EXISTING PAVEMENT, TO PROVIDE A FLATTER DRIVEWAY GRADE, OR TO OTHERWISE IMPROVE THE DRIVE. ALL ADDITIONAL COSTS FOR SUCH WORK SHALL BE THE RESPONSIBILITY OF THE PROPERTY OWNER AND SHALL NOT BE A PART OF THIS PROJECT.
9. DRIVEWAY WIDTH DENOTED AS "W" ON THE DETAIL DRAWINGS SHALL BE A MINIMUM OF 12' (FEET) AND A MAXIMUM OF 50% OF PLATTED LOT WIDTH, UNLESS THE LOT IS ZONED AS MULTI-FAMILY.
10. CONTRACTOR SHALL BE RESPONSIBLE FOR ADA COMPLIANCE FOR SIDEWALK SECTIONS THROUGH DRIVES. ANY SECTION FOUND TO BE NON-COMPLIANT AFTER CONSTRUCTION SHALL BE REMOVED AND REPLACED WITHIN ADA ALLOWANCES BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
11. SUBSTITUTE DOWELS WITH EXPANSION JOINT WHERE CONCRETE CURB AND GUTTER IS TIED TO MAIN LINE PAVING.



VALLEY CENTER
STANDARD DETAILS

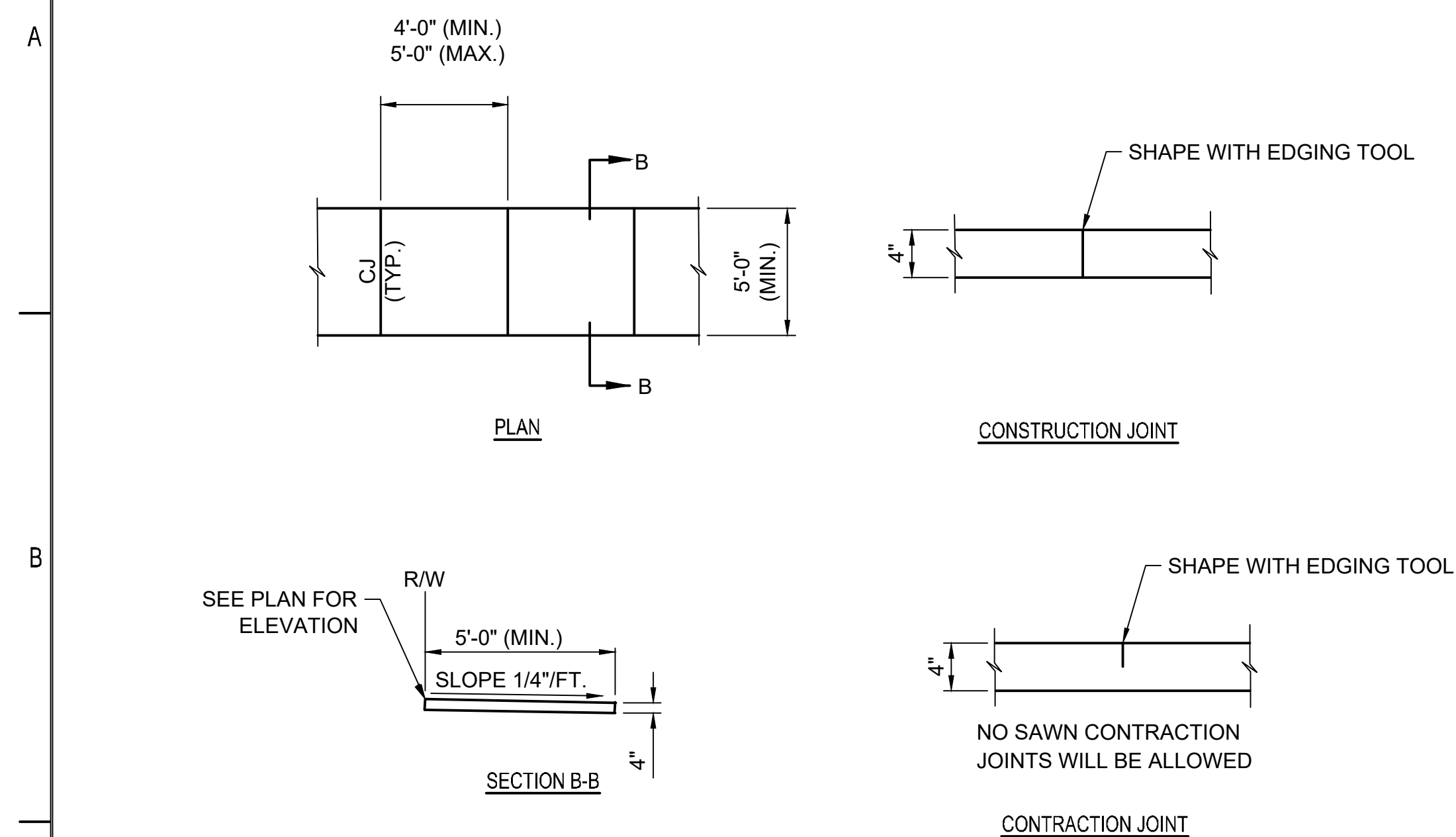
VALLEY CENTER, KS

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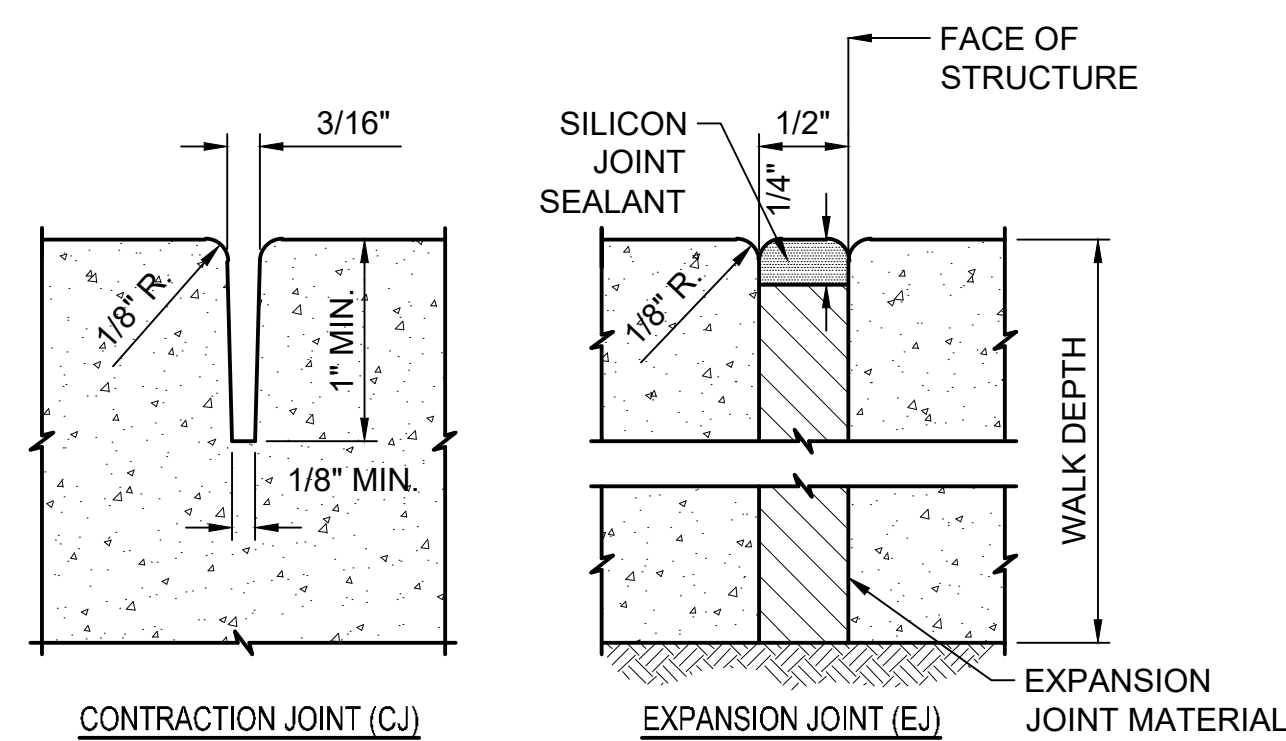
JOB NO.	217013-001
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REINFORCED CONCRETE DRIVE DETAILS

C-504



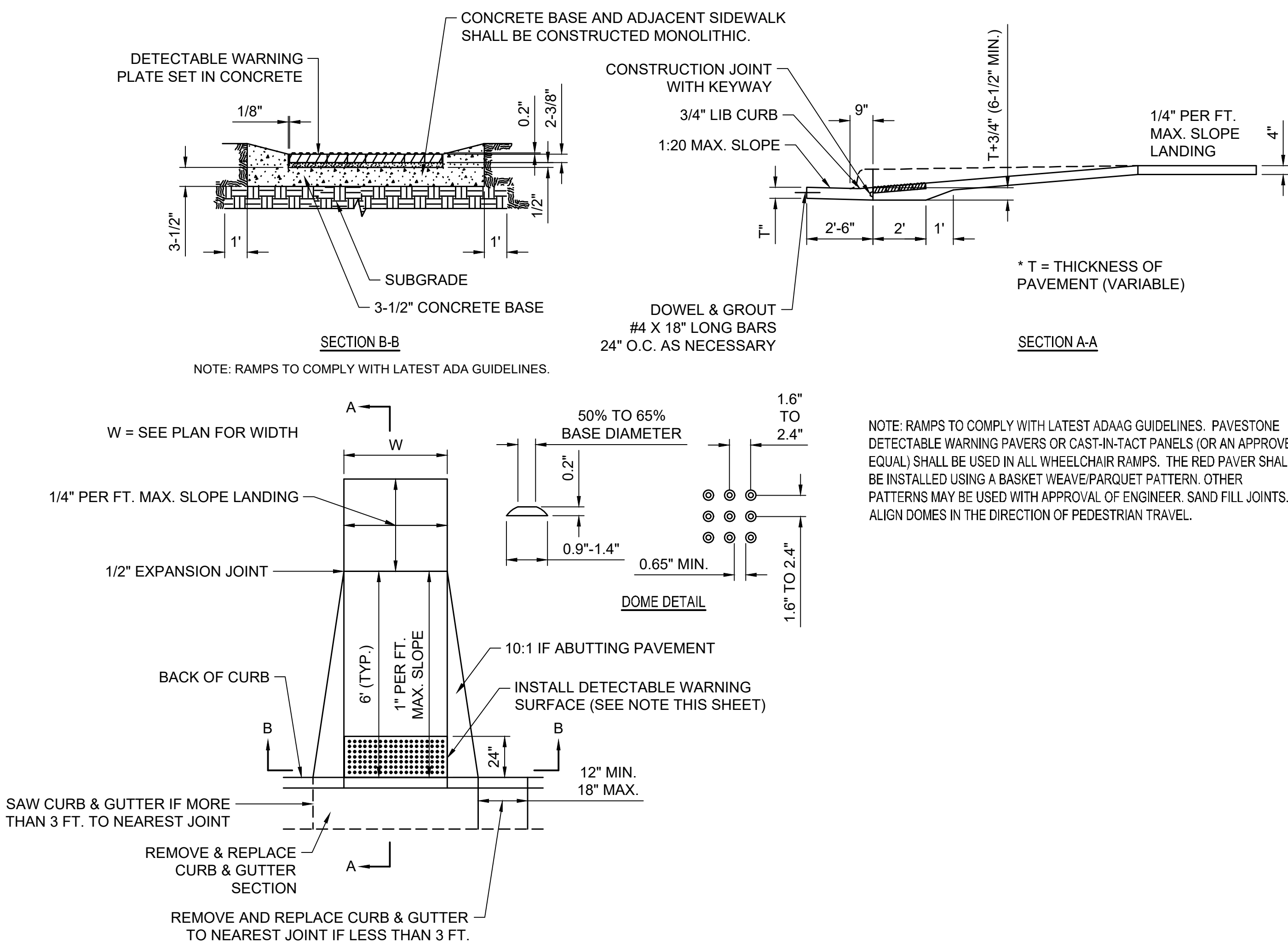
C1 4" WALK DETAILS
NOT TO SCALE



CONCRETE WALK JOINT NOTES

1. PROVIDE 1/2" EXPANSION JOINT MATERIAL AT ALL LOCATIONS WHERE NEW CONCRETE ABUTS EXISTING OR NEW STRUCTURES AND AT BACK OF CURB.

E1 CONCRETE WALK JOINT DETAILS



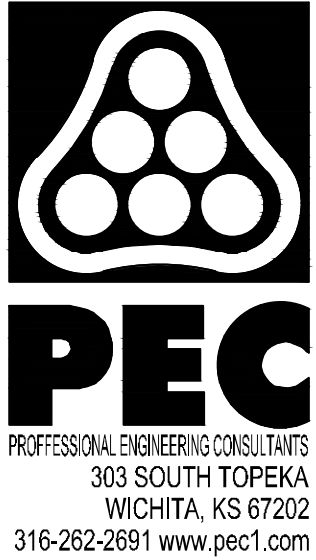
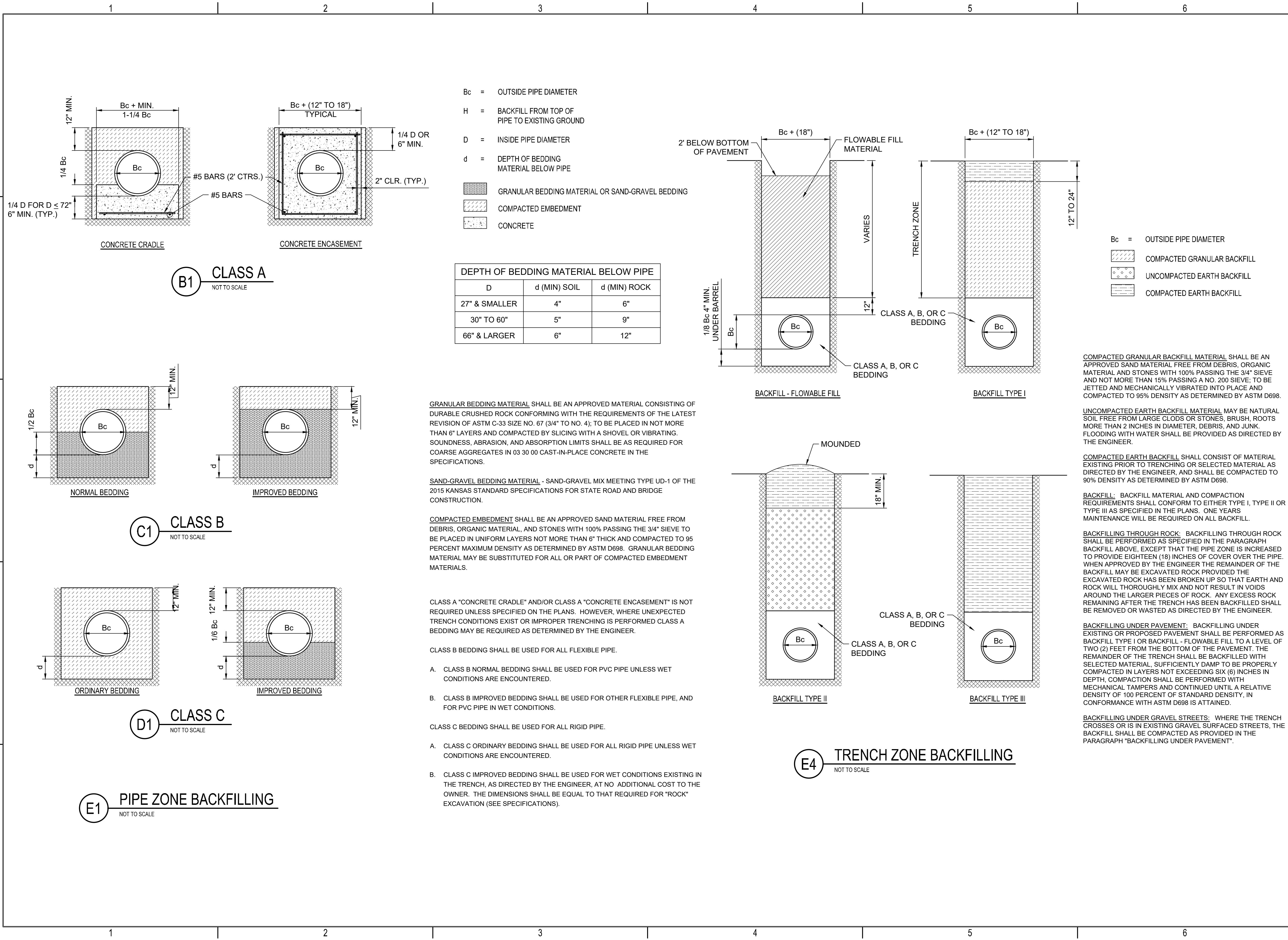
D4 STANDARD ACCESSIBLE RAMP
NOT TO SCALE

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SIDEWALK DETAILS

C-505

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DETAILS.DWG



VALLEY CENTER
STANDARD DETAILS
VALLEY CENTER, KS

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BEDDING & BACKFILL DETAILS

C-506

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STORM SEWER PRECAST
CONCRETE MANHOLE
(CIRCULAR)

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PLOTTED 3/8/2021 9:18:36 AM BY DUSTIN BILLINGSLEY
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CONCRETE MANHOLE (CIRCULAR).DWG



1. GRATE FRAME TO BE INSTALLED ON THIN MORTAR CUSHION TO INSURE FULL SUPPORT ALONG BRICK. CONCRETE USED FOR INLET CONSTRUCTION SHALL CONFORM TO CITY SPECIFICATIONS FOR CONCRETE PAVEMENT MIX.
2. INLET INVERT SHALL BE SHAPED WITH 8 SACK SAND MIX CONCRETE TO CREATE FLOW CHANNELS AND TO INCREASE HYDRAULIC EFFICIENCY SUCH THAT THE INLET WILL BE SELF CLEANING BETWEEN ALL INLET AND/OR OUTLET PIPES.
3. THE ENDS OF ALL PIPES INSTALLED IN INLETS SHALL BE CUT OFF FLUSH WITH THE INSIDE FACE OF THE INLET WALL.
4. INLET FRAME AND GRATE TO BE DEETER #1261 OR EJ1936Z1 OR APPROVED EQUAL.
5. CONTRACTOR SHALL REMOVE LIFTING HOOKS AFTER INSTALLATION. RECESSES IN MANHOLE WALL SHALL BE GROUTED FLUSH TO THE MANHOLE WALL WITH HYDRAULIC CEMENT AFTER THE MANHOLE IS IN PLACE. LIFTING HOLES THRU THE MANHOLE WALL WILL NOT BE ACCEPTED.



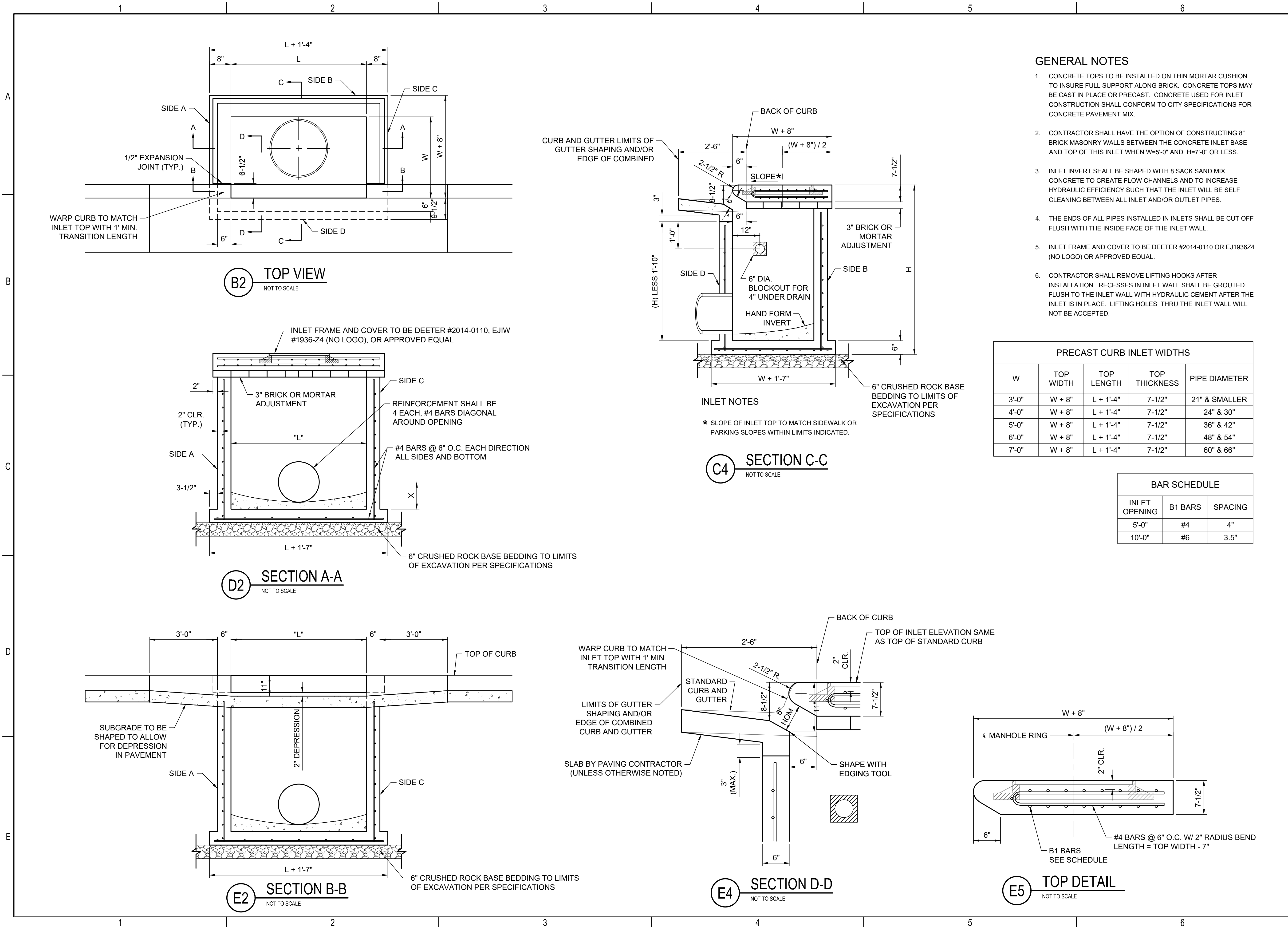
VALLEY CENTER STANDARD DETAILS

VALLEY CENTER, KS

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STORM SEWER REINFORCED CONCRETE MANHOLE (RECTANGULAR)		

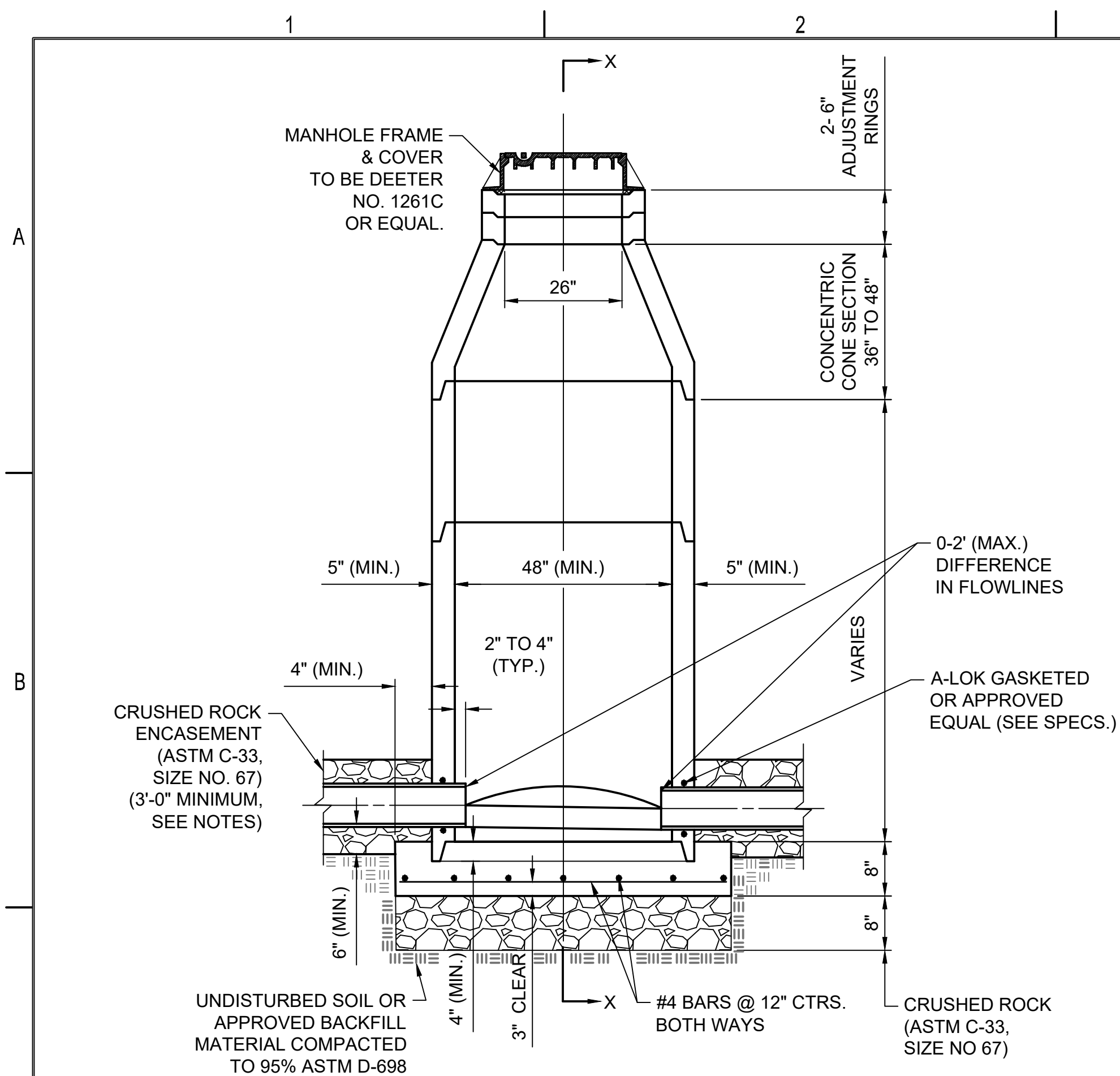
C-508

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DETAILS.DWG

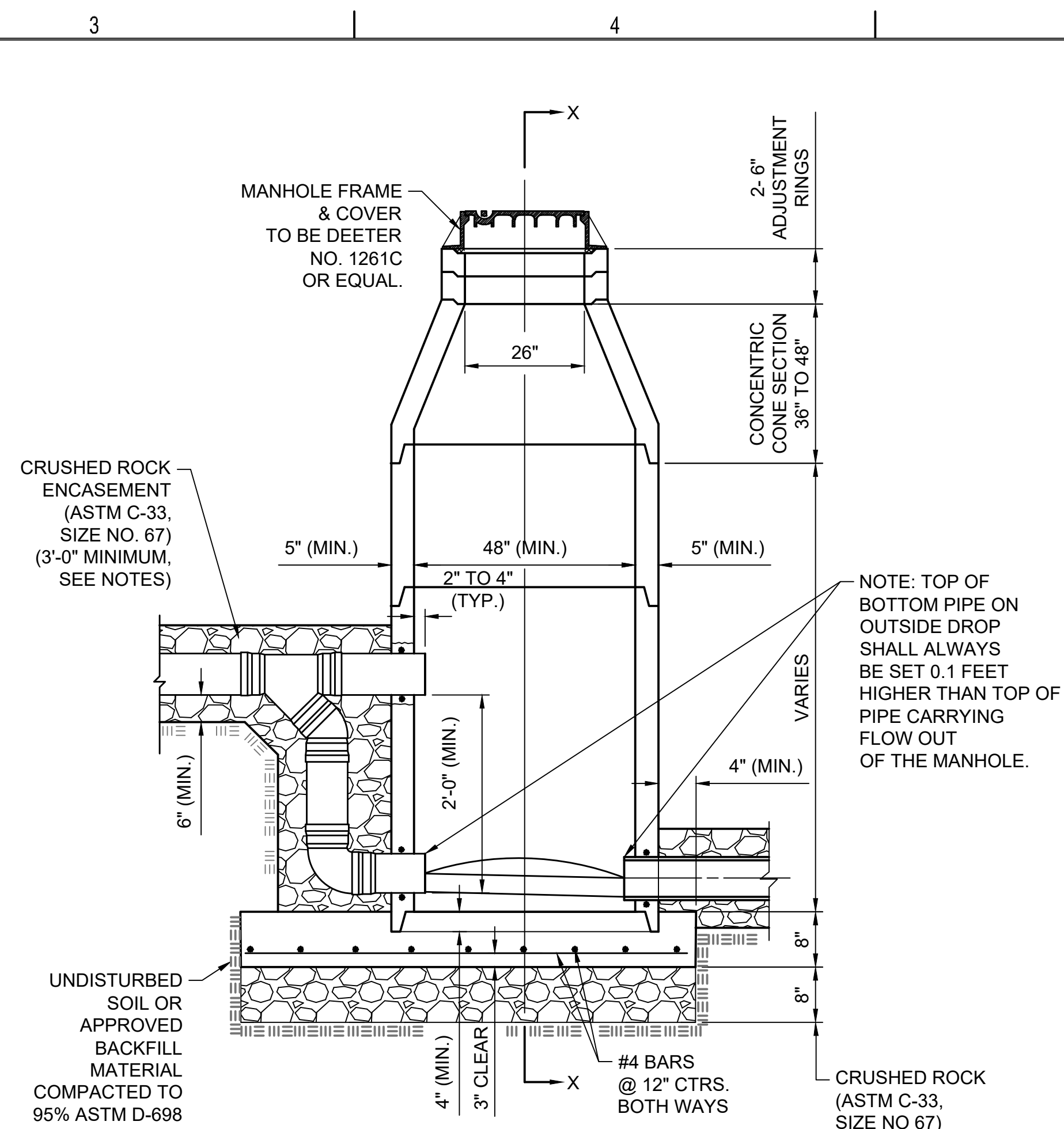


VALLEY CENTER
STANDARD DETAILS
VALLEY CENTER, KS

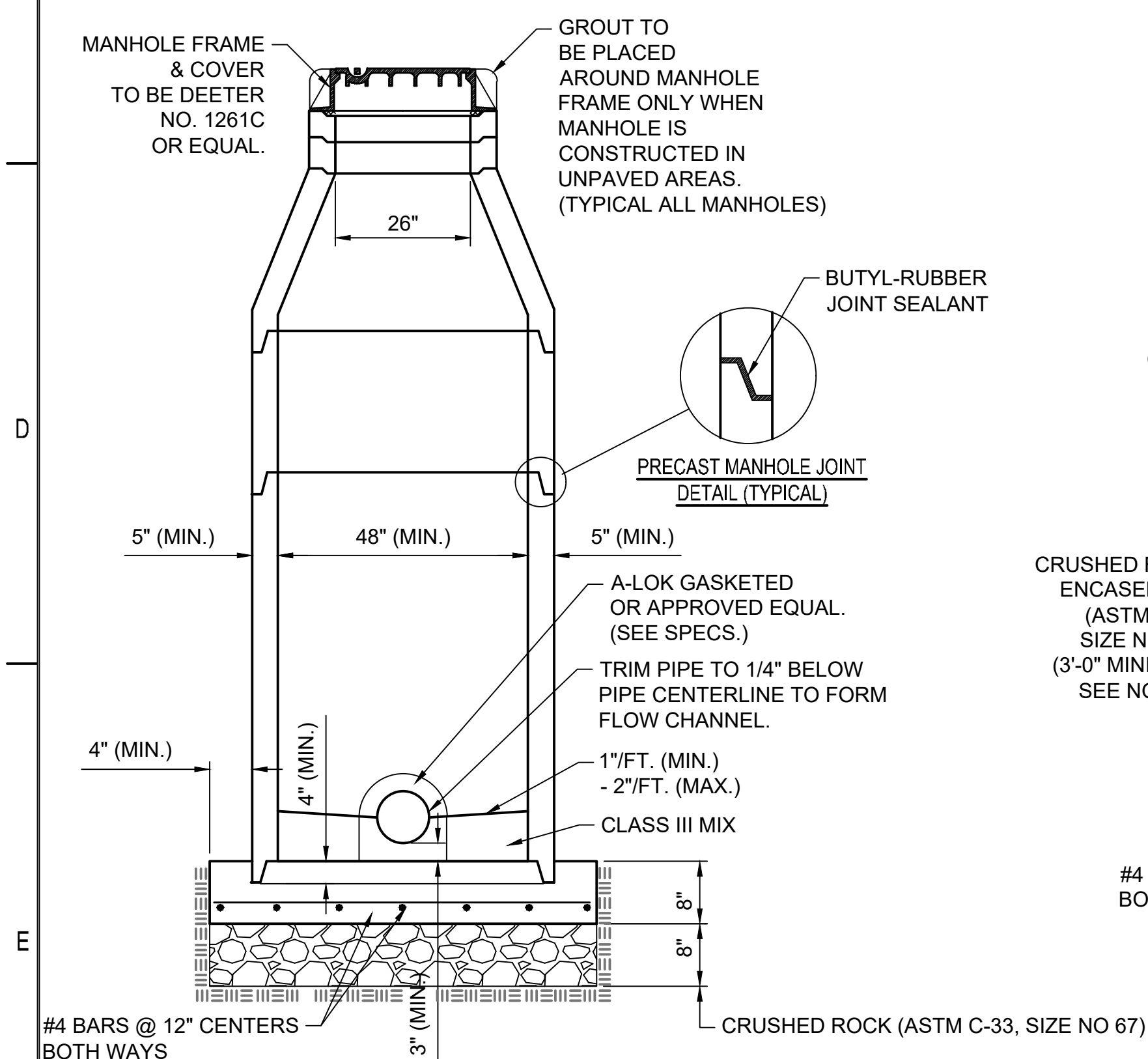
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DESIGNED BY	JAG	
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TYPE 1 CURB INLET DETAILS		
C-510		



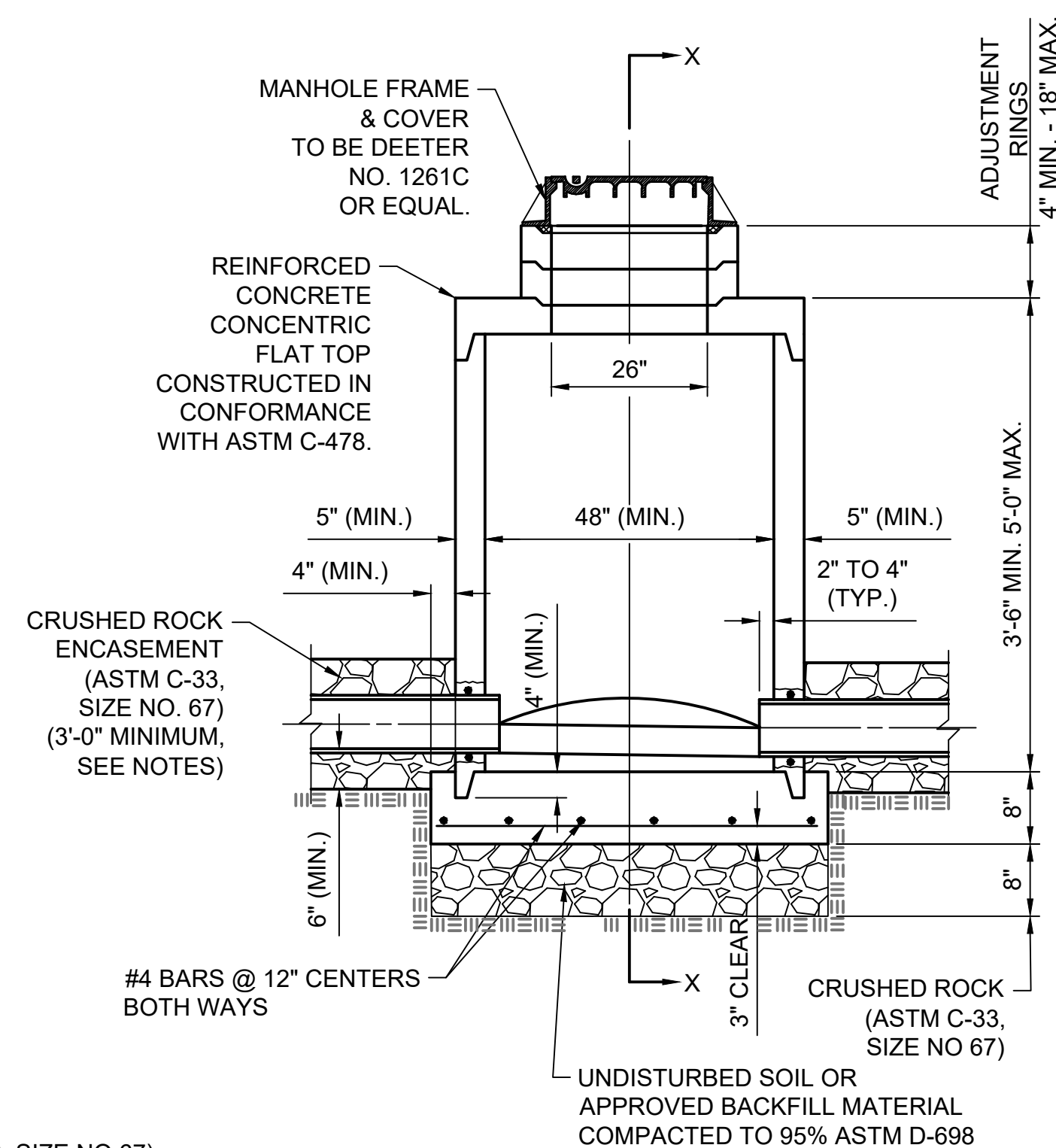
C1 PRECAST STANDARD MANHOLE
NOT TO SCALE



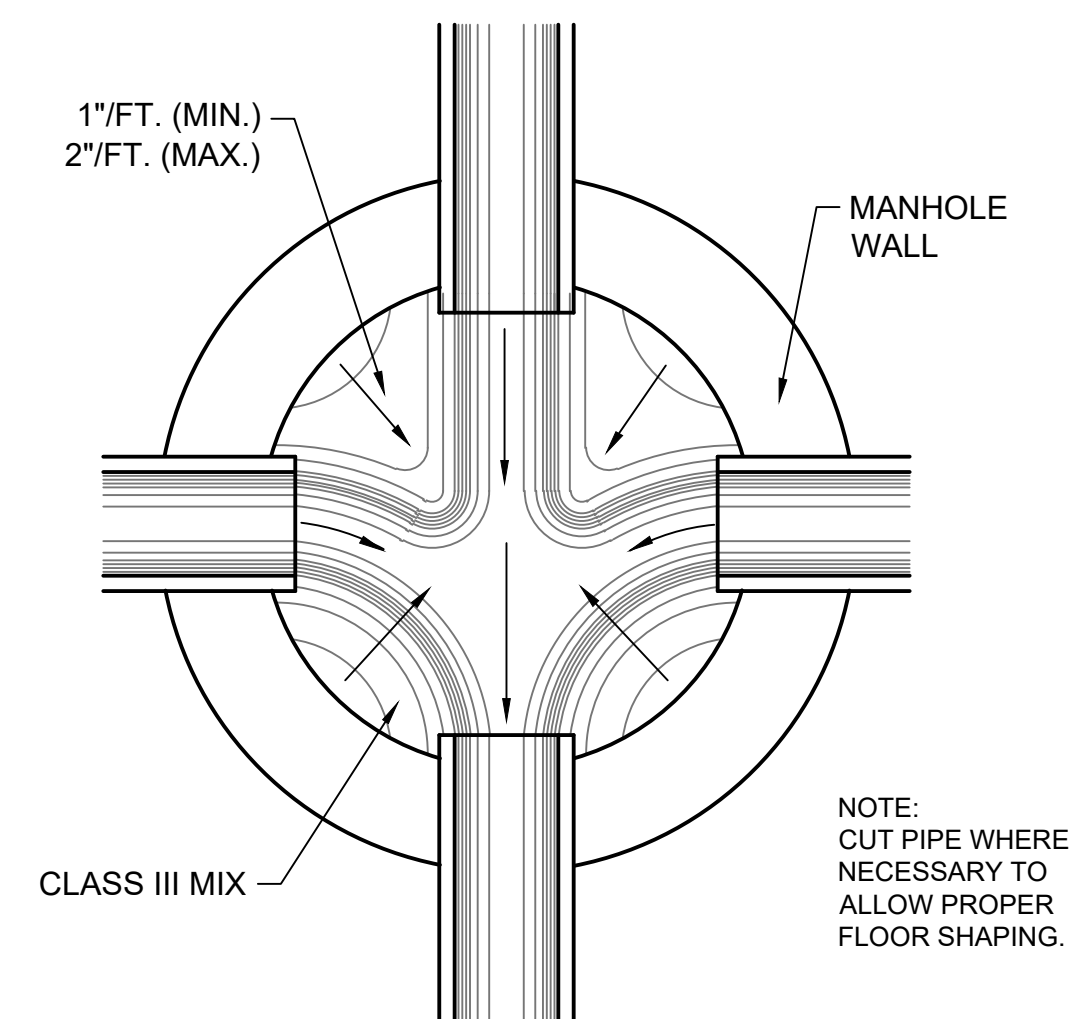
C3 PRECAST OUTSIDE DROP MANHOLE
NOT TO SCALE



E1 SECTION X (TYPICAL)
NOT TO SCALE



E3 PRECAST SHALLOW MANHOLE
NOT TO SCALE



E4 TYPICAL MANHOLE FLOOR SHAPING

PRECAST MANHOLE NOTES

2. IF, IN THE OPINION OF THE ENGINEER, THE MANHOLE SUBGRADE APPEARS UNSTABLE, THE CONTRACTOR WILL HAVE THE OPTION TO COMPACT SUBGRADE AS SHOWN OR INCREASE THE THICKNESS OF THE MANHOLE BASE AS DIRECTED BY THE ENGINEER.
2. STEEL REINFORCING WILL BE REQUIRED IN ALL MANHOLE BASES.
3. DOG HOUSE MANHOLES MAY BE USED WHERE CONNECTING TO EXISTING SEWERS. APPROVED FLEXIBLE WATERSTOP GASKETS WHICH MEET OR EXCEED THE TEST REQUIREMENTS OF ASTM C-923 SHALL BE INSTALLED TO JOIN THE SEWER TO THE MANHOLE WALL WHEN PLASTIC PIPE IS USED. SEWER PIPE EXTENDING FROM MANHOLES SHALL BE SUPPORTED WITH CRUSHED ROCK ENCASEMENT A MINIMUM OF 3 FEET FROM THE MANHOLE WALL. WHEN A-LOK GASKETS ARE NOT PROVIDED FOR THE PIPE PENETRATION TO THE MANHOLE.
4. THE MANHOLE FRAME SHALL BE SEATED ON AN APPROVED BUTYL-RUBBER SEALANT TO PROVIDE A WATER-TIGHT SEAL BETWEEN THE MANHOLE ADJUSTMENT RING AND THE MANHOLE FRAME.
5. GASKETED PIPE PLUGS AND CAPS SHALL BE PROVIDED BY THE PIPE SUPPLIER.
6. ALL MANHOLE CONSTRUCTION SHALL BE WATER TIGHT.
7. TOP OF MANHOLE FLOOR SLAB SHALL BE AT LEAST 3 INCHES BELOW THE FLOW LINE OF THE OUTLET PIPE TO INSURE SUFFICIENT MINIMUM THICKNESS OF SHAPED INVERT.
8. ALL PRECAST CONCRETE MANHOLE SECTIONS SHALL CONFORM TO THE LATEST REVISION OF ASTM C-478 AS MODIFIED BY THE SPECIFICATIONS.
9. CONCRETE FOR MANHOLE BASES SHALL BE CLASS I AS DESCRIBED IN THE SPECIFICATIONS.
10. PRECAST MANHOLES SHALL BE SET AT LEAST 4 INCHES INTO MANHOLE BASE. CAST IN PLACE BASES SHALL BE 8" THICK (MIN.) WITH #4 BARS @ 12" CENTERS.
11. MANHOLES WITH PIPE SIZES 24" AND LARGER SHALL HAVE 5 FOOT INSIDE DIAMETER (MIN.)
12. INSIDE DIAMETER OF FIVE-FOOT DIAMETER PRECAST MANHOLES SHALL REMAIN CONSTANT TO THE LOCATION OF THE REDUCING FLAT TOP WHICH CONNECTS THE FOUR-FOOT DIAMETER CONE SECTION TO THE FIVE FOOT DIAMETER MANHOLE BARREL.
13. ALL INTERIOR CONCRETE SURFACES ABOVE THE BENCH OF PRECAST MANHOLES SHALL BE COATED PER THE STANDARD SPECIFICATIONS.
14. EXTERIOR MANHOLE WALLS SHALL BE COATED WITH 1 COAT VALSPAR HI-BUILD BITUMINOUS COATING 35-J-10, ULTRA-SHIELD WB COATING, BY GMX, TNE MEC 46-450 HEAVY TNE MECOL, OR APPROVED EQUAL.
15. MANHOLES WITH PRECAST BASES MAY BE USED AT THE CONTRACTORS OPTION. MANHOLES WITH PRECAST BASES SHALL HAVE A-LOK OR APPROVED EQUAL GASKETS CAST INTO THE MANHOLE WALL FOR ALL PIPE PENETRATIONS. THESE MANHOLES SHALL HAVE AN 8" MINIMUM BASE THICKNESS AND SHALL BE PLACED ON AN 8" MIN. CRUSHED ROCK BASE. PIPES SHALL BE ENCASED WITH CRUSHED ROCK TO AT LEAST 3 FEET FROM THE MANHOLE WALL. CRUSHED ROCK SHALL MEET THE REQUIREMENTS FOR GRANULAR BEDDING MATERIAL.
16. MANHOLE SECTIONS SHALL BE SUPPLIED WITH RECESSED LIFTING EYES. LIFTING EYE RECESSES SHALL BE GROUTED FLUSH TO THE MANHOLE WALL WITH HYDRAULIC CEMENT AFTER THE MANHOLE IS IN PLACE. LIFTING HOLES THRU THE MANHOLE WALL WILL NOT BE ACCEPTED.

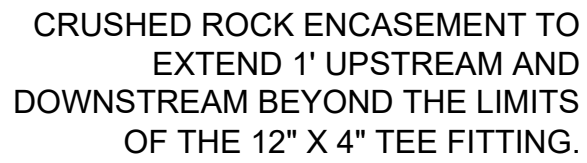


VALLEY CENTER
STANDARD DETAILS

VALLEY CENTER, KS

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C-514



NOT TO SCALE



NOT TO SCALE

1. CRUSHED ROCK SHALL MEET THE REQUIREMENTS FOR GRANULAR BEDDING MATERIAL, AS OUTLINED IN THE SPECIFICATIONS.
2. SERVICE CONNECTIONS ARE TO BE INSTALLED WHERE PROPOSED SEWER MAIN IS 10' OR MORE BELOW PROPOSED GROUND OR AS SHOWN IN THE PLANS.



NOT TO SCALE

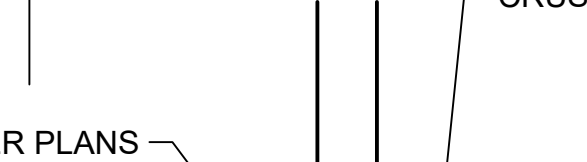
(PLUGGING & STAKING SHALL BE SUBSIDIARY TO
THE LUMP SUM PRICE BID FOR SITE RESTORATION)



— STAKED TO SUPPORT IN PLACE AS APPROVED BY THE ENGINEER.

SERVICE CONNECTIONS TYPE 1 ARE TO BE INSTALLED WHERE PROPOSED SEWER MAIN IS 12' OR MORE BELOW PROPOSED GROUND OR AS SHOWN IN THE PLANS.

СГСТ



NOT TO SCALE



NOT TO SCALE

[illegible]

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C-515



VALLEY CENTER STANDARD DETAILS

VALLEY CENTER, KS

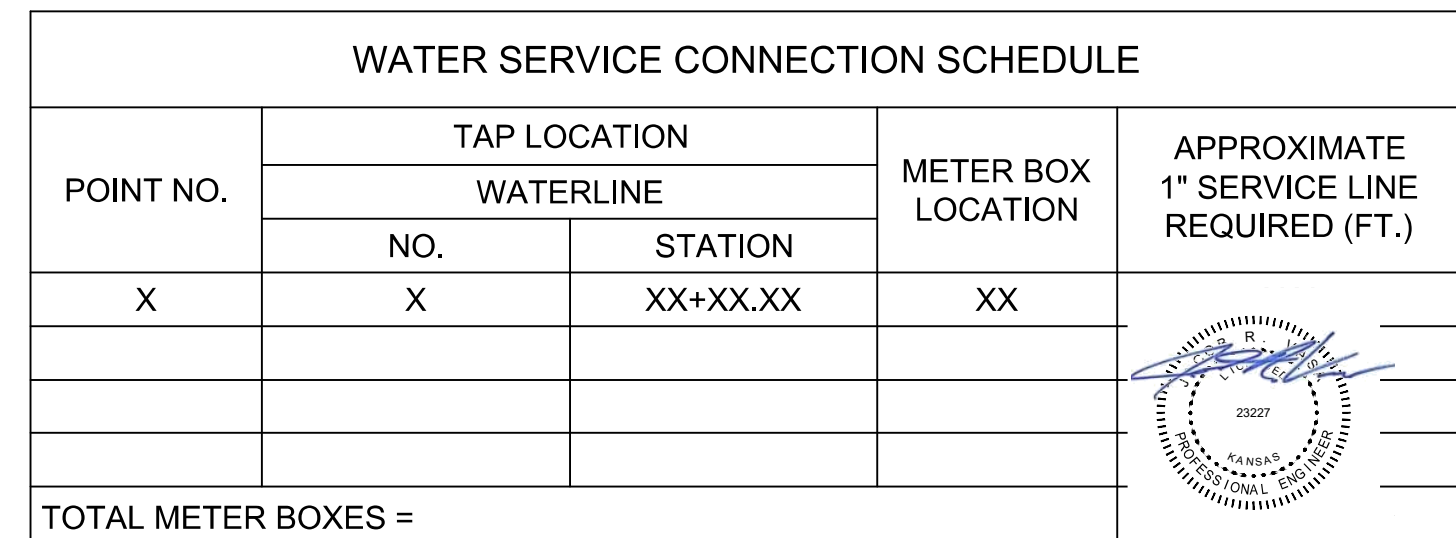


Diagram illustrating the proposed waterline layout. The waterline is shown as a dashed line with a 5'-0" offset from the centerline. The minimum width of the waterline is 10'-0", and the minimum depth from the centerline is 3'-0".

C4 PROTECTIVE FILL DETAIL
NOT TO SCALE



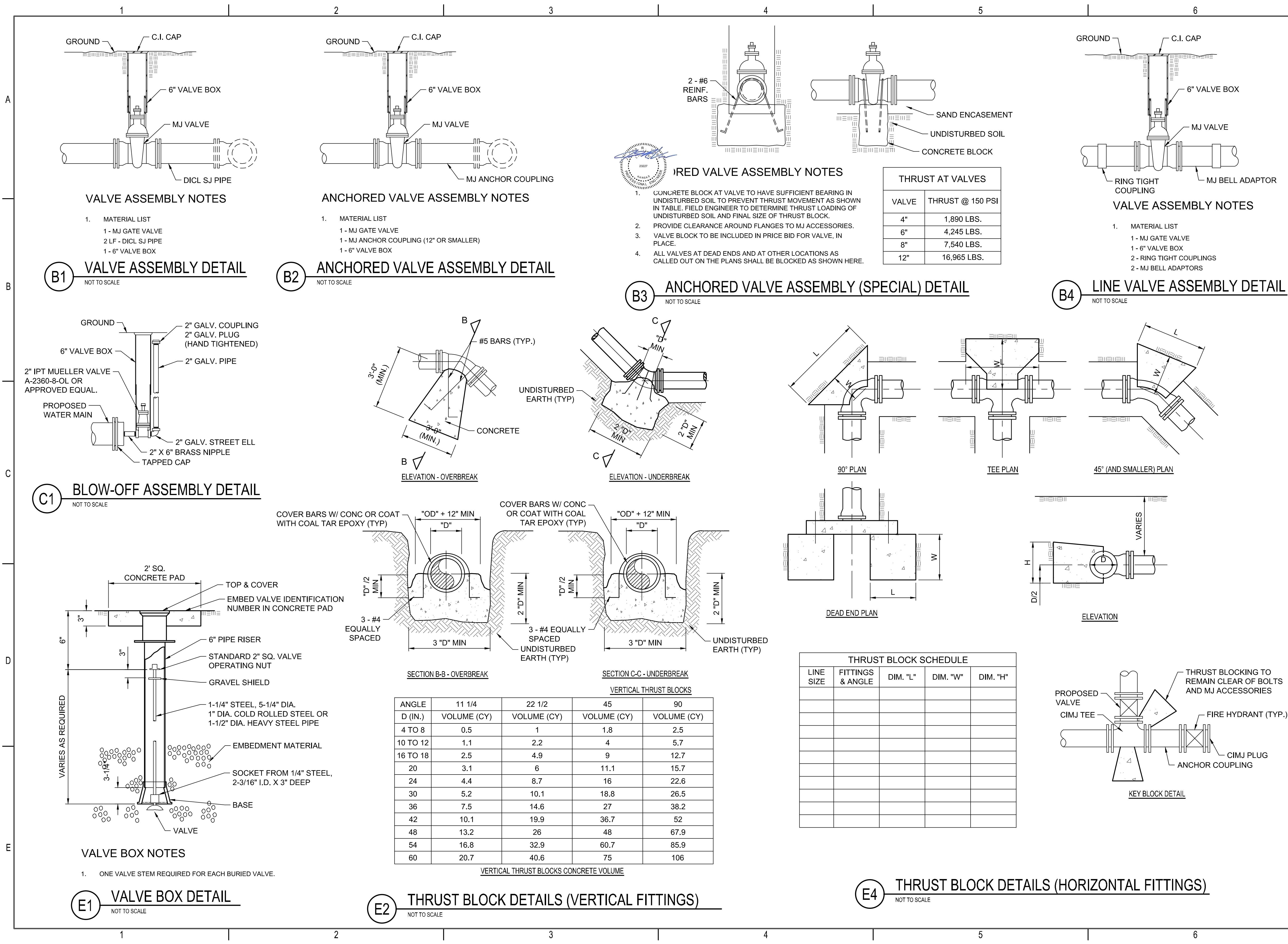
E3 AIR RELEASE AND COMBINATION AIR/VACUUM RELEASE ASSEMBLY DETAIL

[illegible]

C5 FIRE HYDRANT ASSEMBLY
NOT TO SCALE



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U:\WICHITA-CIVIL\2021\217013001\MUNIDRAWINGS\STANDARD DETAILS\217013-001-C-517-WATERLINE DETAILS
2.DWG



VALLEY CENTER
STANDARD DETAILS
VALLEY CENTER, KS

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WATERLINE DETAILS 2

